

MONARO HIGHWAY

PROPOSED ROCK QUARRY

PROPOSED INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY



LOCALITY PLAN







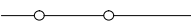




PART INDEX

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













ORIGINAL DRAWING IN COLOUR

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| | | | | | Prepared by | | Client | | | | | |
| | | | | |  | |   | | Drawn A.D. | | Designer A.D | Client - |
| 03 ISSUED FOR INFORMATION | | A.D. | 20.02.19 | Check C.N. | | | | | Check C.N. | Project PROPOSED ROCK QUARRY | | |
| 02 ISSUED FOR INFORMATION | | A.D. | 11.12.18 | | | | | Approved - | | Title INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY | | |
| 01 ISSUED FOR INFORMATION | | A.D. | - | - | 12.10.18 | | | Date - | | COVER SHEET | | |
| No. | Revision Description | | Initials | Approved | Date | | | Scale AS SHOWN | | Sheet A1 Drawing No: DC18020-OP2-CS-1100 Rev: 03 | | |
| | | | | | | | | | | DO NOT SCALE | | |
| Dimensions in metres unless otherwise noted | | | | | | | | | | | | |

LEGEND (GENERAL)

| | |
|---|---|
|  | EXISTING PAVEMENT |
|  | TEMPORARY PAVEMENT |
|  | EXISTING FEATURES |
|  | TEMPORARY FRANGIBLE BOLLARDS |
|  | TEMPORARY BARRIER (TL-3 OR EQUIVALENT) |
|  | TEMPORARY WATER FILLED BARRIER |
|  | FENCE |
|  | TEMPORARY BARRIER APPROVED END TREATMENT WITH HAZARD MARKER |
|  | TEMPORARY CRASH CUSHION WITH HAZARD MARKER |
|  | TEMPORARY PEDESTRIAN ROUTE |
|  | FLOW PATH |

LEGEND (LINEMARKING)

| | |
|---|---------------------------------------|
|  | DOUBLE BARRIER LINE |
|  | ENHANCED DOUBLE BARRIER LINE (0.3m) |
|  | ENHANCED DOUBLE BARRIER LINE (0.8m) |
|  | LEFT HAND EDGE LINE |
|  | LEFT HAND EDGE LINE - PACIFIC HIGHWAY |
|  | RIGHT HAND EDGE LINE |
|  | LANE LINE |
|  | UNBROKEN LANE LINE |
|  | CONTINUITY LINE |
|  | TRAFFIC ISLAND OUTLINE EDGE LINE |
|  | CHEVRON OUTLINE EDGE LINE |
|  | GIVE WAY LINE |
|  | LEFT TURN LANE RURAL - AR3(L) |
|  | RIGHT TURN LANE RURAL - AR3(R) |
|  | LANE CHANGE RURAL - ARR5 |
|  | STRAIGHT AHEAD - AR1 |

LEGEND (SIGNS)

| | |
|--|-------------------------------|
|  | EXISTING SIGN LOCATION |
|  | NEW SIGN LOCATION |
|  | NEW GUIDE POST |
|  | NEW SIGN |
|  | EXISTING SIGN |
|  | EXISTING SIGN TO BE RELOCATED |
|  | EXISTING SIGN TO BE REMOVED |
|  | EXISTING SIGN TO BE COVERED |

GENERAL

1. ALL DIMENSIONS ARE IN METRES.
2. LEVELS ARE IN METRES WITH RESPECT TO THE AUSTRALIAN HEIGHT DATUM (AHD).
3. NORTH REPRESENTS MAP GRID OF AUSTRALIA (ZONE 56) ORIENTATION.
4. ALL SURFACE WATER FLOWS TO BE MANAGED ALONG WITH CONSTRUCTION SITE WATER USING TEMPORARY DRAINAGE MEASURES. TEMPORARY 'SO KERB' LONGITUDINAL PIT/PIPE/HEADWALLS AS PER DRAINAGE PLAN. SURFACE DRAINAGE TO MEET RMS G10 CLAUSE 2.7.3 - 1 IN 5 YEAR OCCURRENCE STORM.
5. PAVEMENT DESIGN ON PORTION A (DESIGN LOT A-87-TS-03) DRAWING SET, DRAWING DS2016/000619 SHEET TS-A-33204 ISSUE 'A' : EDMS NO. SF2015/118045 (ISSUED FOR CONSTRUCTION BY OTHERS) HAS BEEN ADOPTED FOR THIS PACKAGE OF WORKS.
6. FOUNDATION TREATMENTS TO BE IN ACCORDANCE WITH RMS R44. FOUNDATION TREATMENTS TO BE GENERALLY AS PER THE PERMANENT WORKS FOUNDATION DESIGN. I.E. TEMPORARY WORKS THAT FALL UNDER THE FOOTPRINT OF THE PERMANENT WORKS ARE SUBJECT TO ON SITE CONFIRMATION BY THE GEOTECHNICAL REPRESENTATIVE. TREATMENTS OUTSIDE THE PERMANENT WORKS FOOTPRINT ARE SUBJECT TO ONSITE DETERMINATION BY THE GEOTECHNICAL REPRESENTATIVE.
7. EROSION AND SEDIMENTATION CONTROL TO BE IN ACCORDANCE WITH THE PROJECT ENVIRONMENTAL MANAGEMENT PLAN & PROGRESSIVE EROSION & SEDIMENT CONTROL PLAN (PESCP) FOR THE AREA.

BARRIERS

1. BARRIER END TREATMENTS ARE TO BE INSTALLED IN ACCORDANCE WITH THE RMS LIST OF APPROVED/ACCEPTED TEMPORARY SAFETY BARRIER END TREATMENT LIST.
2. THE EXTENT OF THE SITE OF WORKS IS NOT KNOWN AND LOCATIONS OF ENDS OF BARRIERS ARE APPROXIMATE ONLY. AS THE SITE OF THE WORKS MAY VARY DURING THE PERIOD OF USE OF THIS TEMPORARY WORKS PLAN THE REQUIRED LENGTH OF NEED AND THE LOCATION OF THE ENDS OF BARRIERS MUST BE DETERMINED FOR EACH CHANGE TO THE SITE OF THE WORKS. CONSIDERATION SHOULD BE GIVEN TO THE NEED TO PROTECT WORKERS BY THE PROVISION OF A CONTAINMENT FENCE AT A LATERAL CLEARANCE EQUAL TO THE DYNAMIC DEFLECTION OF THE BARRIER IN A CRASH AND WHERE GATING END TERMINALS ARE USED TO DEFINE THE RUN-OUT AREA.
3. CONTRACTOR TO ENSURE THE DRAINAGE SLOTS IN ALL TEMPORARY CONCRETE BARRIERS ARE KEPT FREE DRAINING.

LINEMARKING

1. REMOVE ALL REDUNDANT LINEMARKING AS STIPULATED IN THE G10 ITEM 4.2.2.
2. ALL GUIDEPOSTS, LINEMARKING & RRPM'S TO BE IN ACCORDANCE WITH THE AOP2742, RMS DELINEATION MANUAL, TRAFFIC CONTROL AT WORK SITES MANUAL & THE RMS QA SPECIFICATION.
3. BB1 & BB2 ENHANCED DELINEATION LINES TO BE INSTALLED IN ACCORDANCE WITH SECTION 5 OF THE RMS DELINEATION MANUAL.
4. LINEMARKING SETOUT INFORMATION TO BE PROVIDED BY STREETWISE ROAD SAFETY & TRAFFIC SERVICES PTY LTD IN ELECTRONIC FORMAT ONCE THE FINAL APPROVED DRAWINGS HAVE BEEN CERTIFIED AND ISSUED FOR CONSTRUCTION.

SERVICES

1. CONTRACTOR TO DETERMINE THE LOCATION, DEPTH AND EXTENT OF UTILITY SERVICES PRIOR TO COMMENCEMENT OF ANY WORK ON THE SITE.
2. NO WORK TO BE CARRIED OUT WITHIN 3m OF ANY EXISTING SERVICES WITHOUT PRIOR RECORDED CONSULTATION WITH THE RELEVANT AUTHORITY.
3. UTILITY INFORMATION SHOWN ON THE PLANS AS PROVIDED BY BGC CONSTRUCTION.
4. THE PRESENCE OF A UTILITY SERVICE, ITS SIZE AND LOCATION SHOULD BE CONFIRMED BY FIELD INSPECTION, PRIOR TO THE COMMENCEMENT OF ROADWORKS AND THE RELEVANT UTILITY PLANS OBTAINED BY DIALING PH 1100 OR FAX 1300 652 077 (DIAL BEFORE YOU DIG). CAUTION SHOULD BE EXERCISED WHEN WORKING IN THE VICINITY OF ALL UTILITY SERVICES.
6. THE CONTRACTOR IS TO CONTACT NETWORK INTEGRITY (1800 810 443) FOR ONSITE LOCATION AND CO-ORDINATION OF TELSTRA UTILITY RELOCATION WORKS.
7. THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR ANY DAMAGE INCURRED TO EXISTING UTILITY SERVICES AS A RESULT OF THE EXECUTION OF THE WORK UNDER THE CONTRACT.

SIGNS

1. SIGNS SHALL BE A MINIMUM OF 0.6m FROM THE EDGE OF FORMATION AND NOT LESS THAN 2m OR MORE THAN 5m FROM THE EDGE OF TRAVEL LANE IN ACCORDANCE WITH AOP2742.2. POST AND GROUND FOOTING DETAILS ARE TO BE SPECIFIED BY CONTRACTOR TO SUIT THE SIZE OF SIGN AND GROUND CONDITION.

ORIGINAL DRAWING IN COLOUR

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Transport
Roads & Maritime
Services

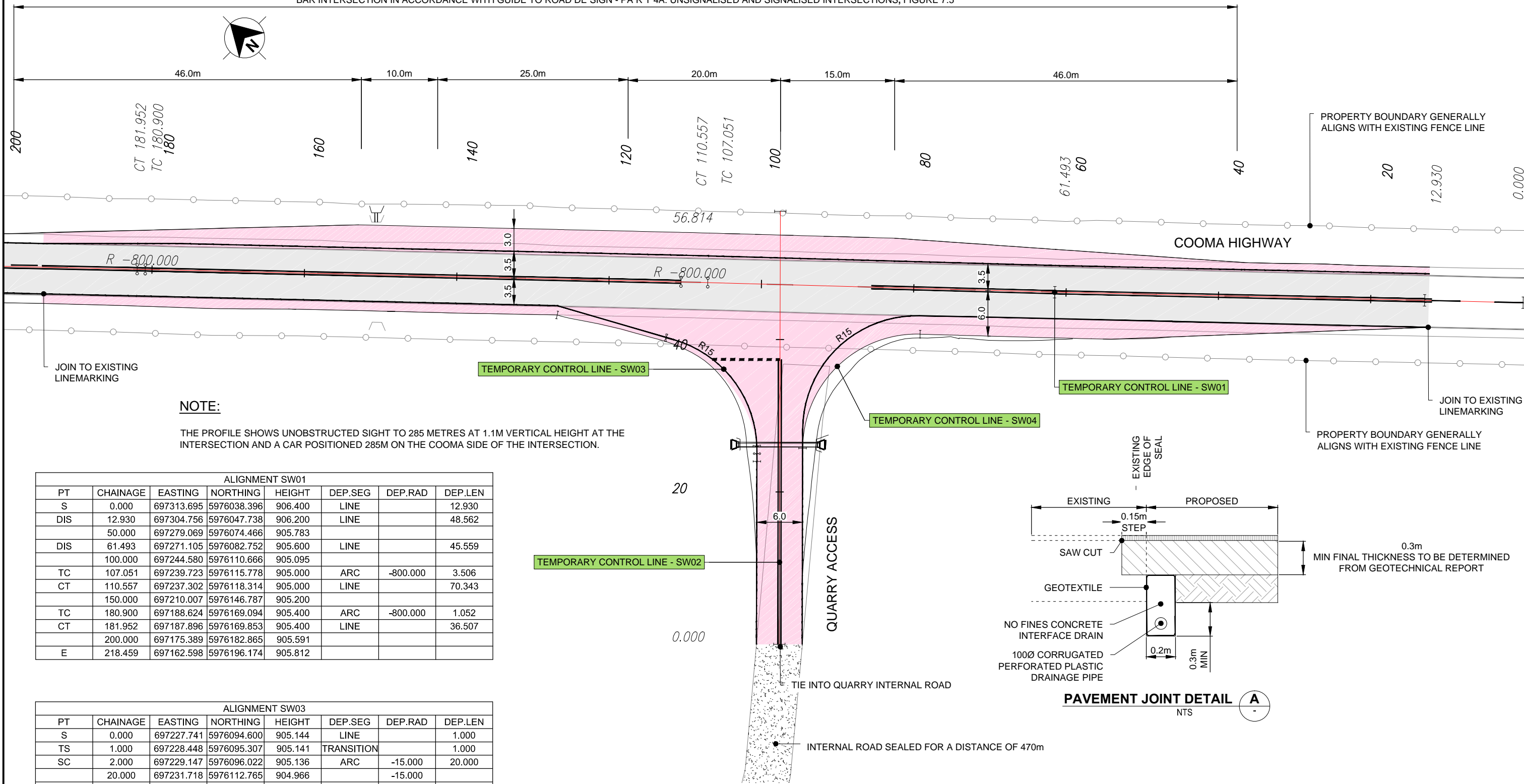
Dimensions in metres unless otherwise noted

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| Approved | - | | |
| Date | - | | |
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DO NOT SCALE

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| Client | - |
| Project | PROPOSED ROCK QUARRY |
| Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY GENERAL NOTES AND LEGEND |
| Sheet | A1 |
| Drawing No: | DC18020-OP2-GN-1101 |
| Rev: | 03 |

BAR INTERSECTION IN ACCORDANCE WITH GUIDE TO ROAD DESIGN - PART 4A: UNSIGNALISED AND SIGNALISED INTERSECTIONS, FIGURE 7.5



NOTES:

- REFER DWG: DC18020-OP2-GN-1101 FOR LEGEND AND NOTES.
- ANY FUTURE SAFETY BARRIERS REQUIRED TO PROTECT ERRANT VEHICLES WILL COMPLY WITH RMS GUIDELINES.

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Transport
Roads & Maritime
Services

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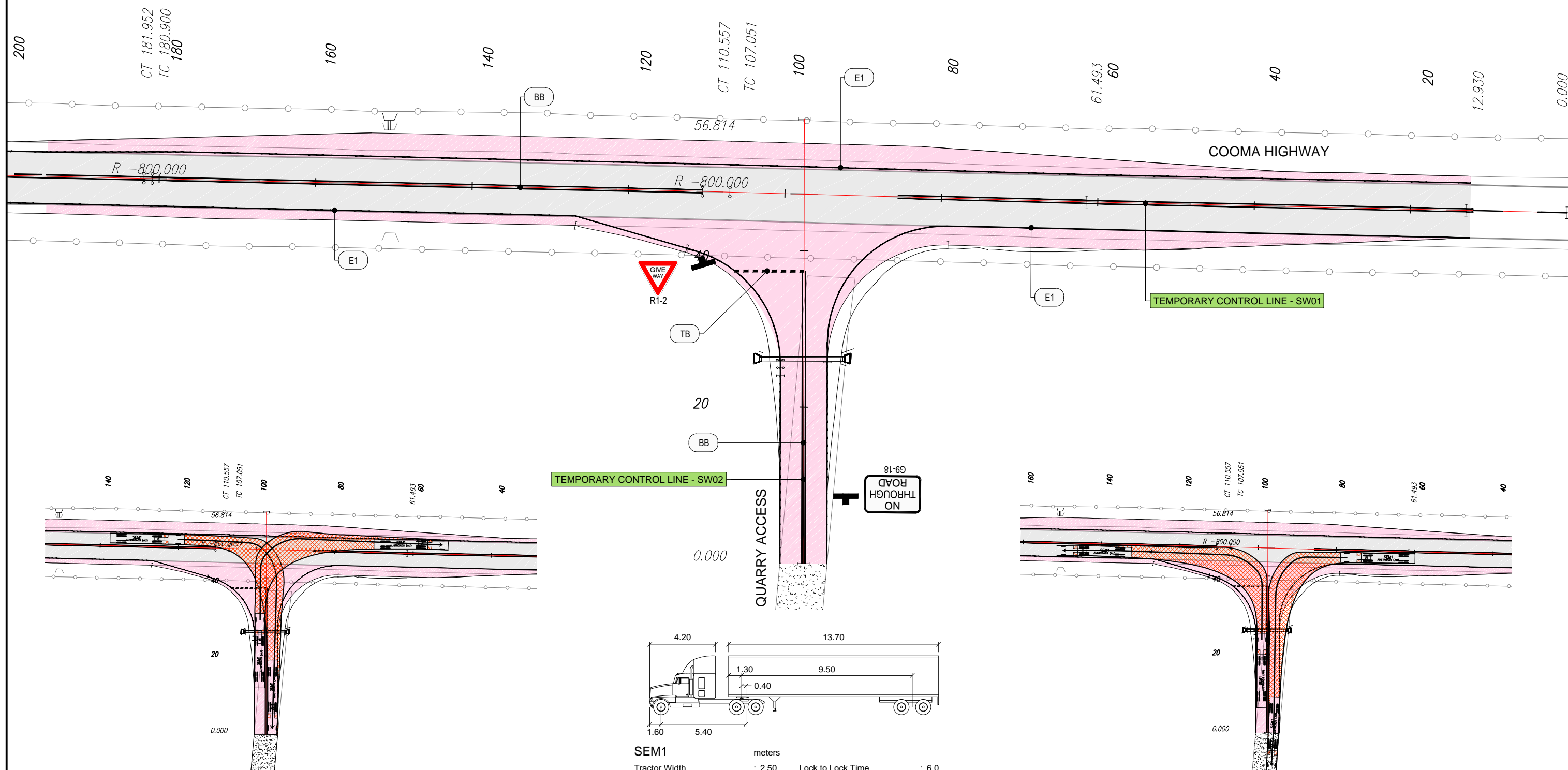
Dimensions in metres unless otherwise noted

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| Check | C.N. | Check | C.N. | Project | PROPOSED ROCK QUARRY |
| Approved | - | | | Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY DETAIL PLAN (1 OF 1) |
| Date | - | | | Sheet | A1 |
| Scale | 1:250 (A1) | | | Drawing No: | DC18020-OP2-DP-1102 |
| | | | | Rev: | 03 |



DESIGN SPEED (COOMA HIGHWAY)



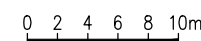
NOTES:

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2. ANY FUTURE SAFETY BARRIERS REQUIRED TO PROTECT ERRANT VEHICLES WILL COMPLY WITH RMS GUIDELINES.

DESIGN VEHICLE

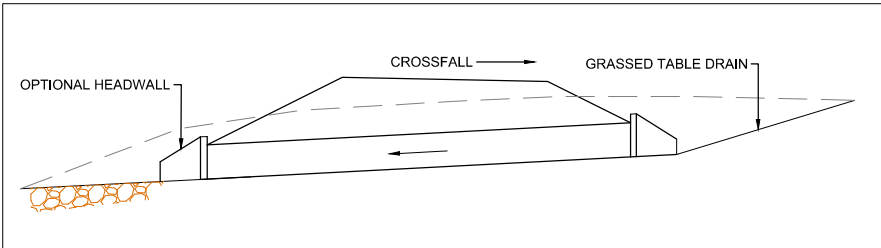
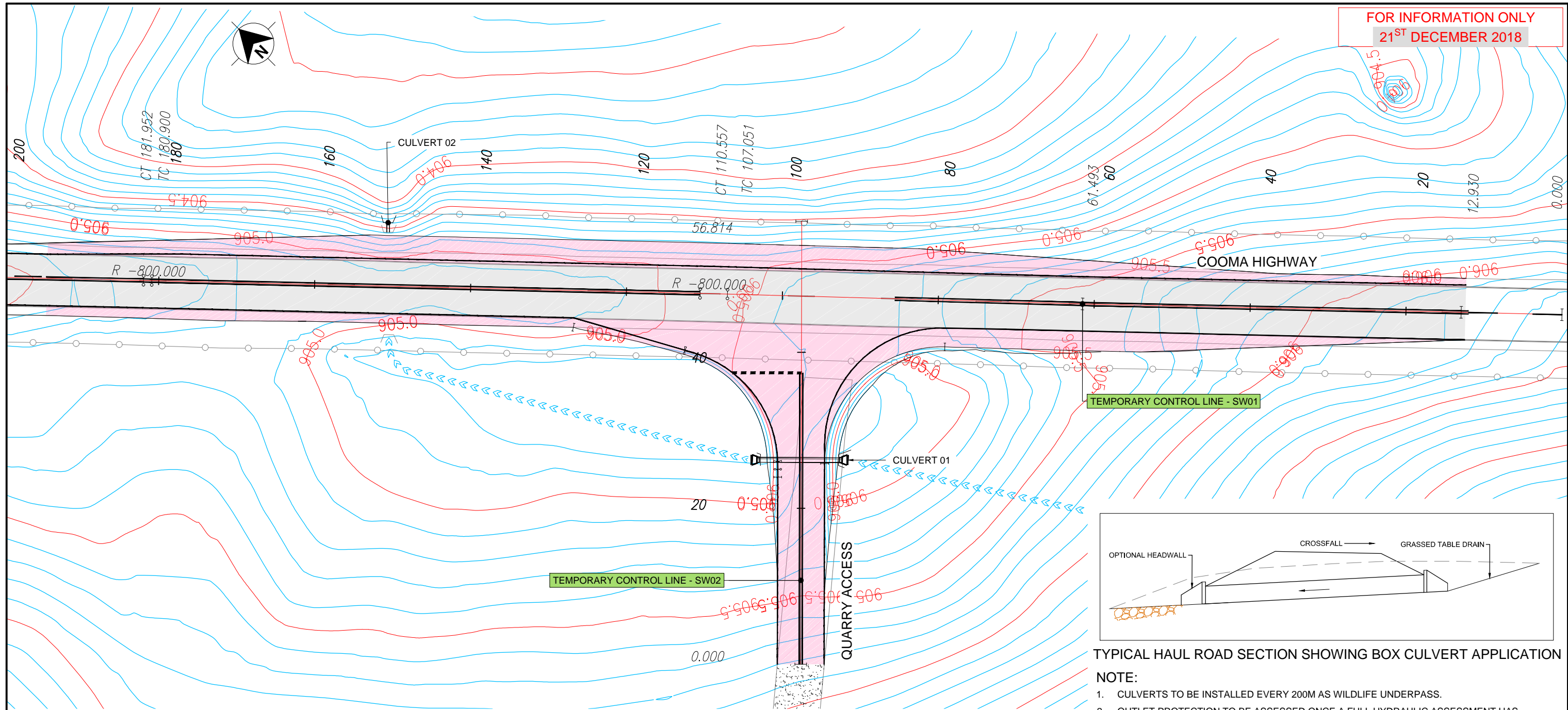
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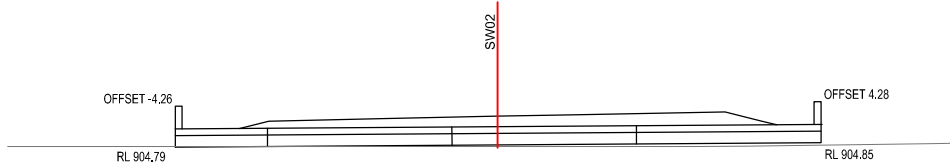
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| Check | C.N. | Check | C.N. | Project | PROPOSED ROCK QUARRY |
| Approved | - | | | Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY SIGNS AND LINEMARKING PLAN (1 OF 1) |
| Date | - | | | Sheet | A1 |
| Scale | 1:250 (A1) | | | Drawing No: | DC18020-OP2-SL-1103 |
| | | | | Rev: | 03 |



TYPICAL HAUL ROAD SECTION SHOWING BOX CULVERT APPLICATION

NOTE:

1. CULVERTS TO BE INSTALLED EVERY 200M AS WILDLIFE UNDERPASS.
2. OUTLET PROTECTION TO BE ASSESSED ONCE A FULL HYDRAULIC ASSESSMENT HAS BEEN UNDERTAKEN AT A FUTURE STAGE.

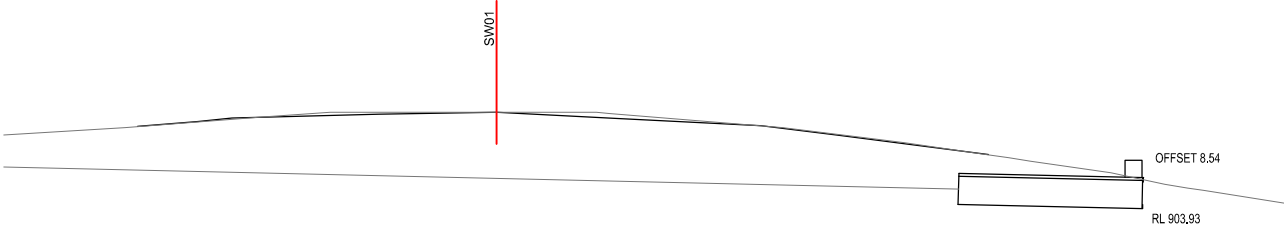


CULVERT 01

300x150 RCBC_PRECAST (1/1.22 3/2.44) (8.54 overall length) 26.2
Base Slab Conc. 1.0m³
Precast Endwalls
C/O Wall Conc. 0.2m³, Headwall Extension Conc. 0.2m³, 22 M12 anchor bars
Backfill Material 9.0m³, Bedding/Haunch Material 0.6m³
External Box Wd. 424mm, Internal Ht. 150mm, Deck Thick. 90mm
Leg Thick. 62mm, Headwall Ht. 300mm, Baseslab Thick. 180mm

NOTE:

1. CULVERT SIZE AND LAYOUT IS CONCEPT ONLY – FURTHER DETAILS WILL BE PROVIDED ONCE A FULL HYDRAULIC ASSESSMENT HAS BEEN UNDERTAKEN.
2. AN ASSESSMENT OF POTENTIAL HAZARDS TO MOTORISTS CREATED BY THE CULVERT, HEADWALLS AND/OR BATTERS WILL BE UNDERTAKEN WHEN THE CULVERT DESIGN IS FINALISED
3. ANY FUTURE SAFETY BARRIERS REQUIRED TO PROTECT ERRANT VEHICLES WILL COMPLY WITH RMS GUIDELINES
4. REFER DWG: DC18020-OP2-GN-1101 FOR LEGEND AND NOTES.

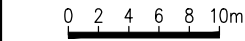


CULVERT 02

375 RCP (1/1.22 6/2.44) (Class 4, Rubber Ring) (15.84 outside of walls) 150.5
Skew Angle 2° Skew Number 88 (2.44 overall length)
External Dia. 445mm, Pipe Thick. 35mm, Headwall Ht. 230mm

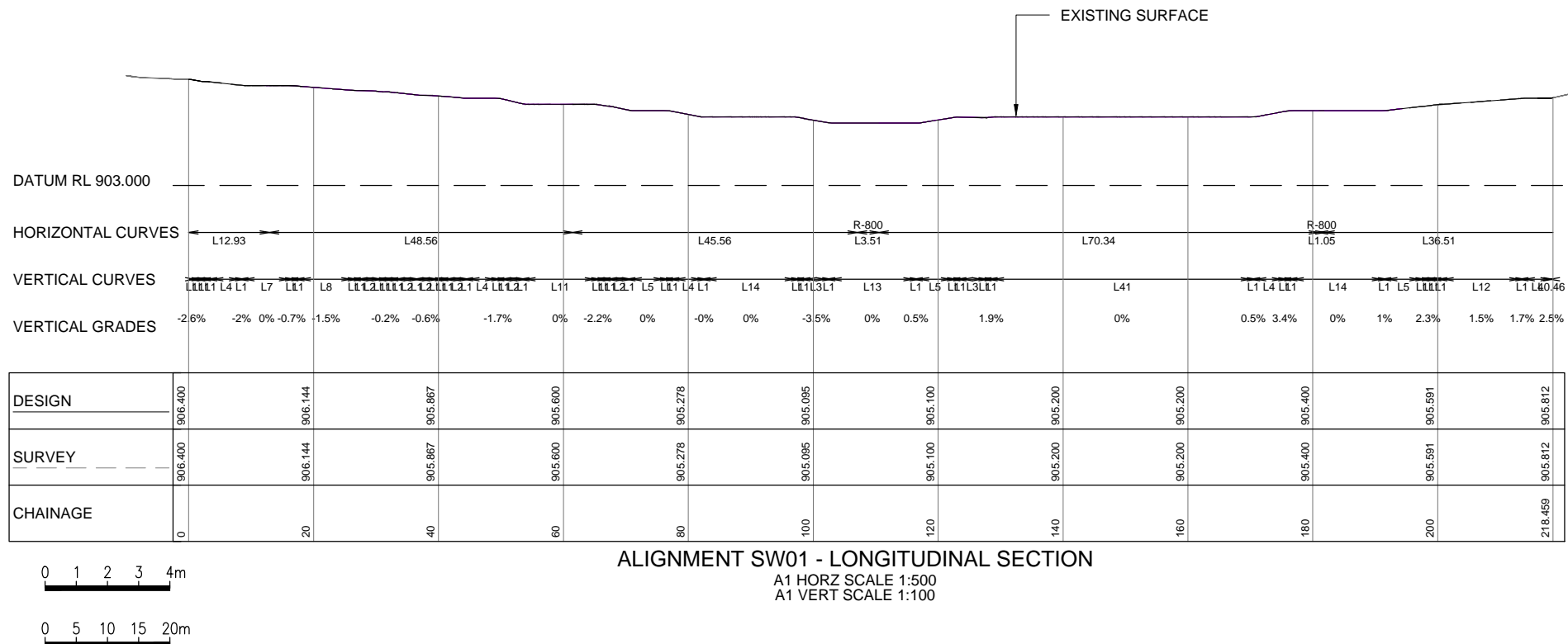
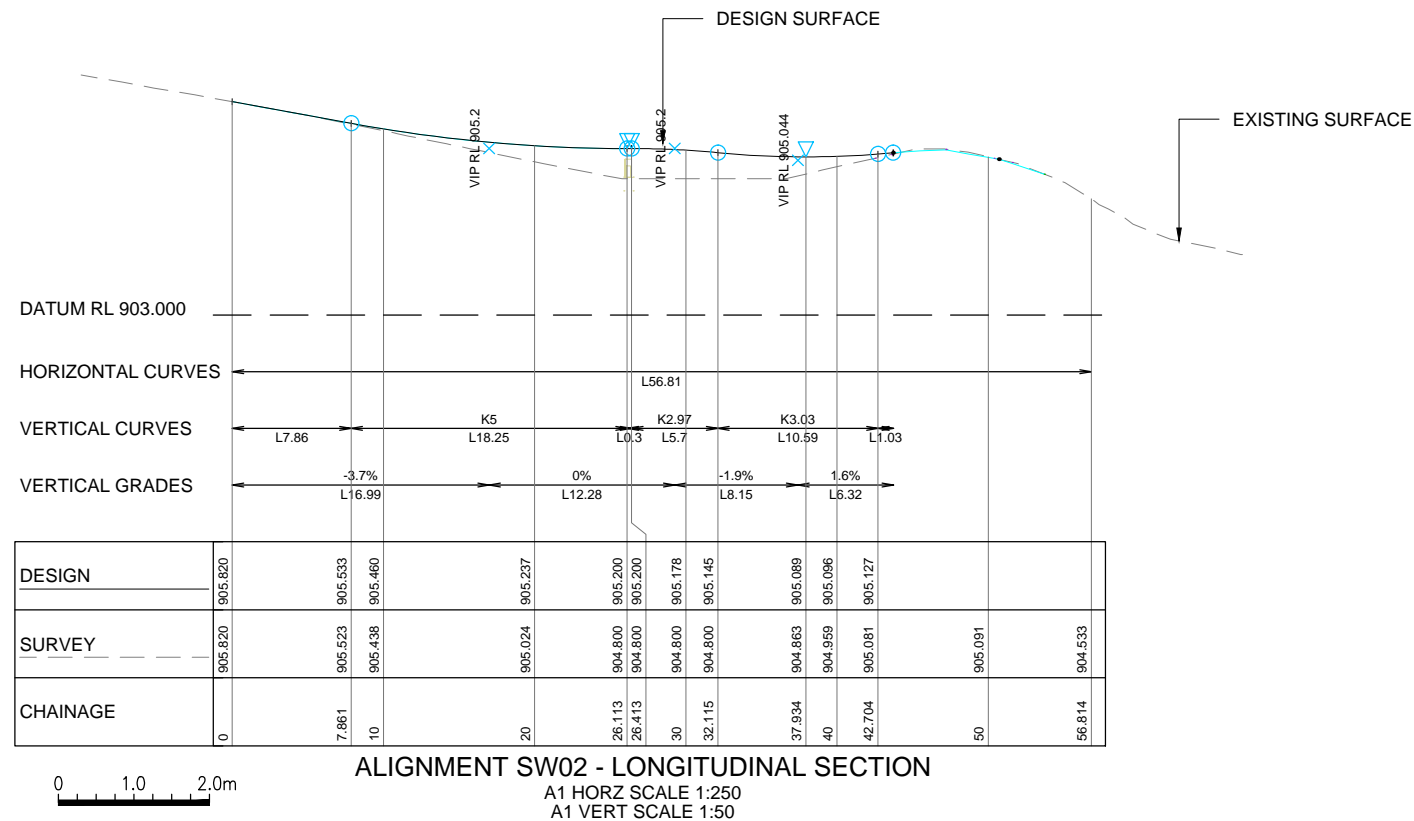
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| Check | C.N. | Check | C.N. | Project | PROPOSED ROCK QUARRY |
| Approved | - | | | Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY DRAINAGE DETAILS (1 OF 1) |
| Date | - | | | Sheet | A1 |
| Scale | 1:250 (A1) | | | Drawing No: | DC18020-OP2-DD-1104 |
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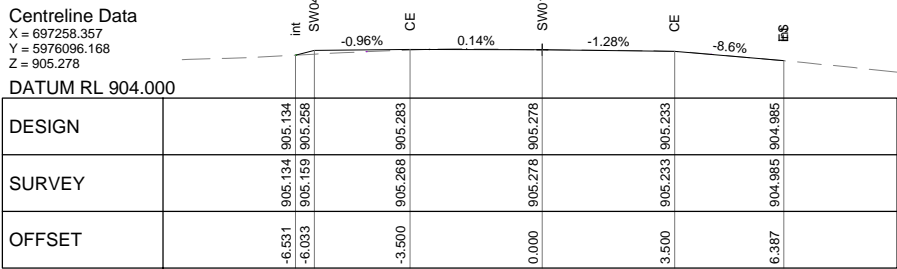
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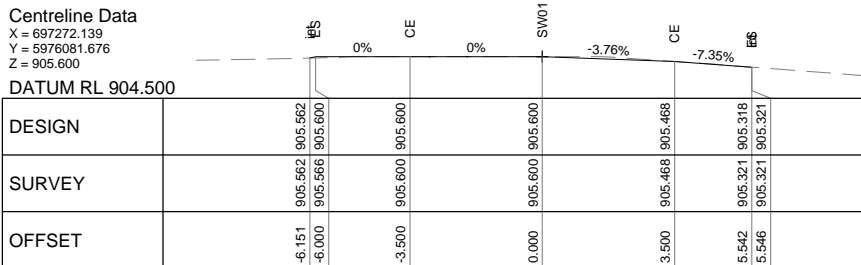
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| Client | - |
| Project | PROPOSED ROCK QUARRY |
| Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY LONGITUDINAL SECTIONS (1 OF 1) |
| Sheet | A1 |
| Drawing No: | DC18020-OP2-LS-1105 |
| Rev: | 03 |

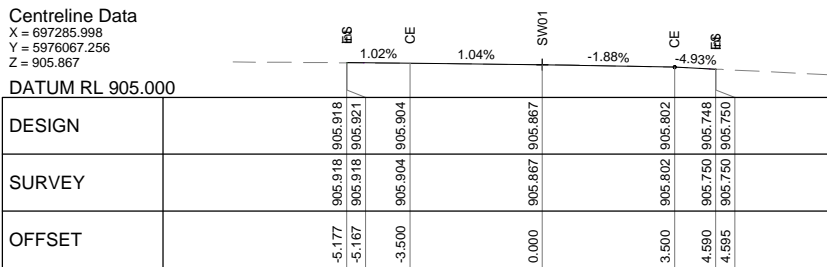
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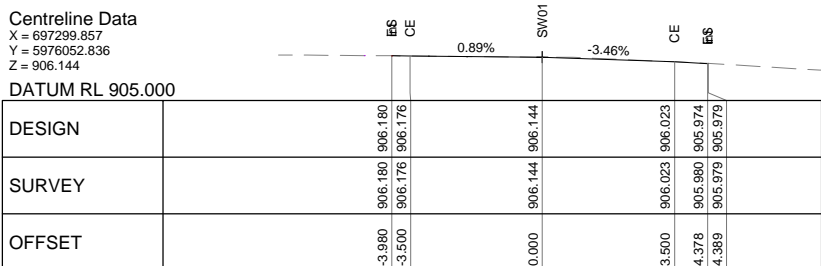
CHAINAGE 80



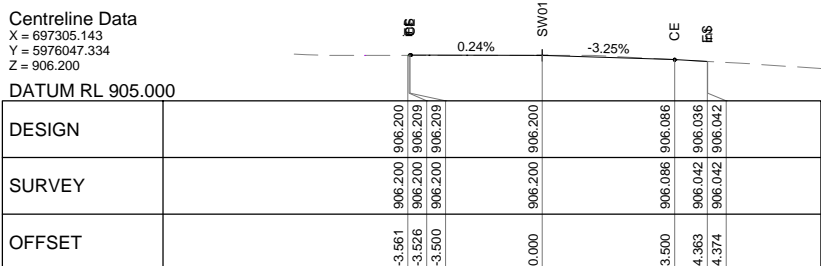
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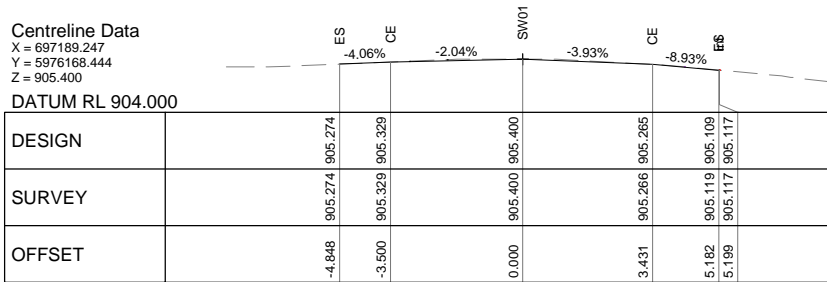
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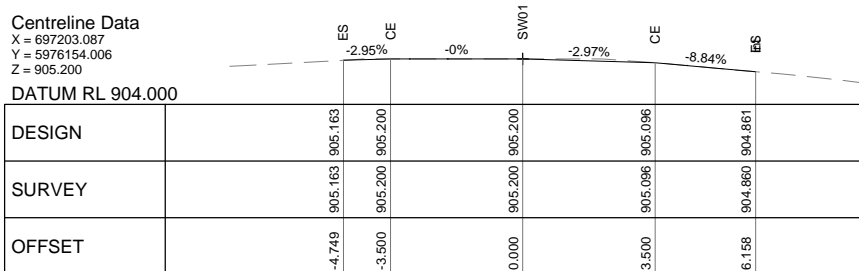
CHAINAGE 2



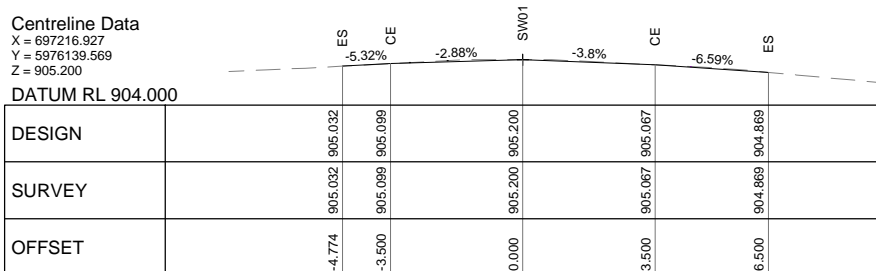
CHAINAGE 12.3



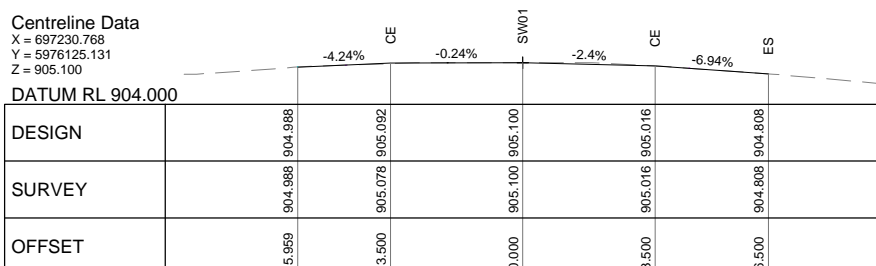
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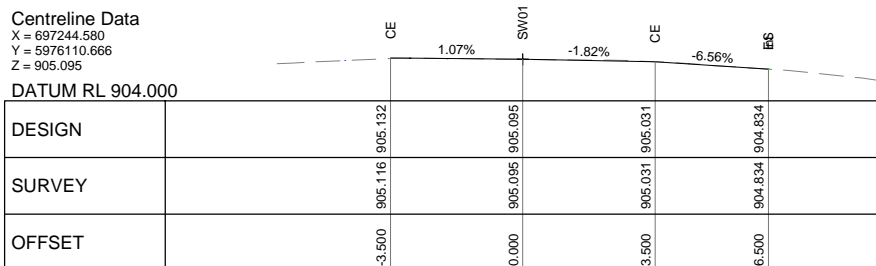
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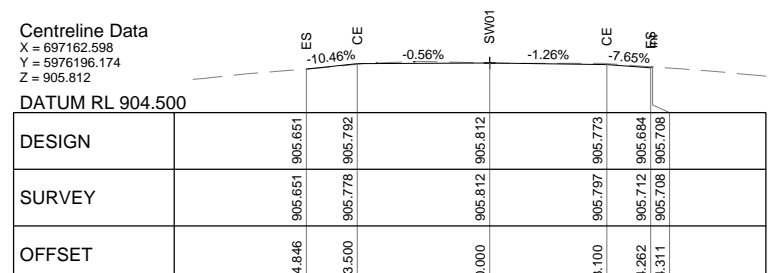
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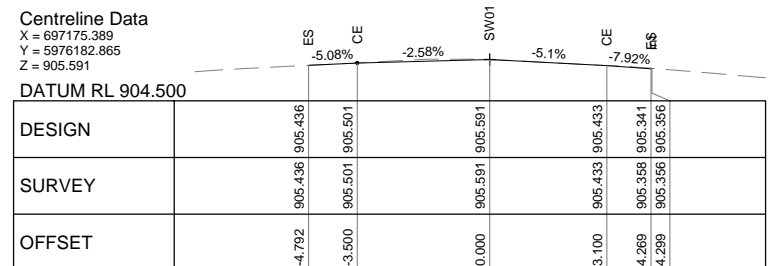
CHAINAGE 12



CHAINAGE 10



CHAINAGE 218 459



CHAINAGE 20

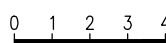
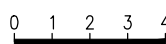
NOTES:

CROSSFALL ON THE EXISTING SHOULDERS ARE -7% TO -11% IN LOCATIONS. DESIGN HAS LIMITED THE CROSSFALL AT THESE TO -3%.

ALIGNMENT - SW01

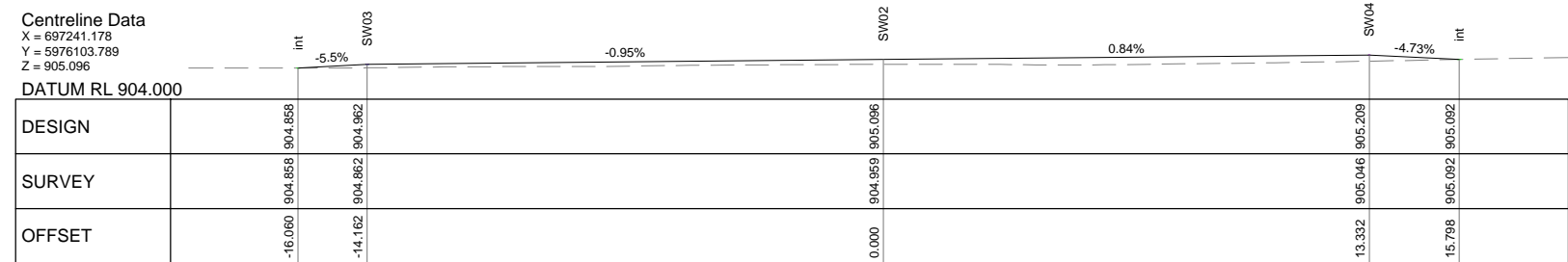
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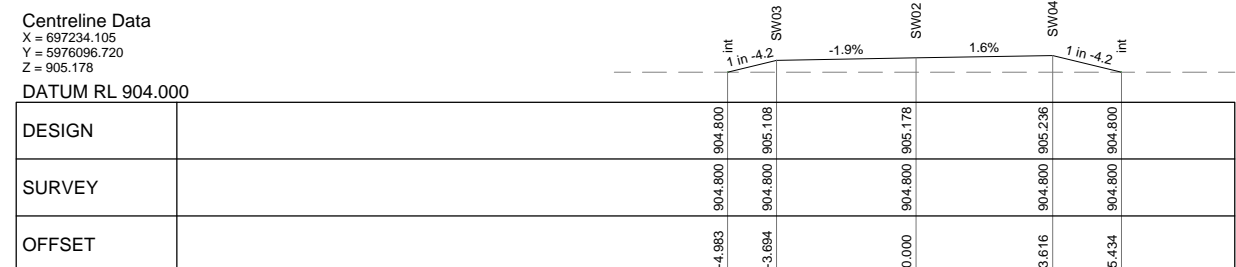


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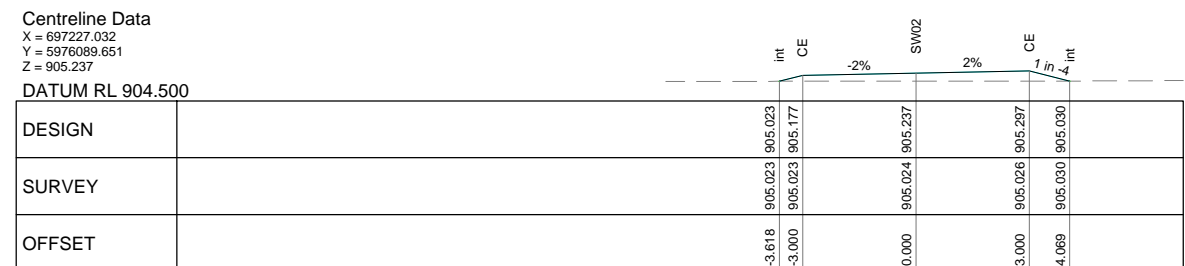
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| Approved | | | | Title | INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY |
| Date | | | | CROSS SECTIONS (1 OF 2) | |
| Scale | 1:100 (A1) | DO NOT SCALE | | Sheet A1 | Drawing No: DC18020-OP2-XS-1106 |
| | | | | | Rev: 03 |



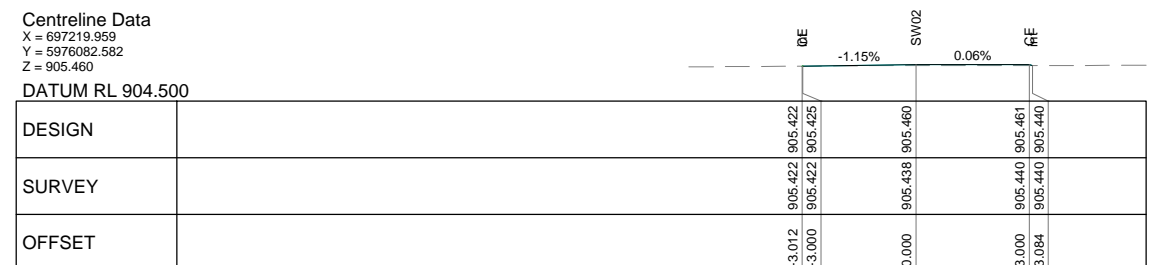
CHAINAGE 40



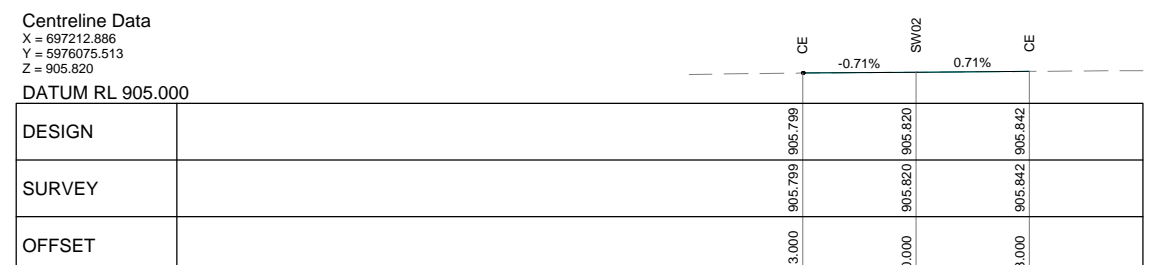
CHAINAGE 30



CHAINAGE 20



CHAINAGE 10



CHAINAGE 0

ALIGNMENT - SW02

ORIGINAL DRAWING IN COLOUR

| | | | | | | | | | | | | | | | |
|-----|------------------------|----------|----------|----------|--|---|---|--|----------|------------------------------|---|----------|---------------------------------|---------|--|
| | | | | | Prepared by | Client | <div>Drawn A.D. Designer A.D</div> <div>Check C.N. Check C.N.</div> <div>Approved -</div> <div>Date -</div> <div>Scale 1:100 (A1)</div> | | Client - | Project PROPOSED ROCK QUARRY | Title INTERSECTION AND QUARRY ACCESS OFF MONARO HIGHWAY CROSS SECTIONS (2 OF 2) | Sheet A1 | Drawing No: DC18020-OP2-XS-1107 | Rev: 03 | |
| | | | | |  |  | <div>0 1 2 3 4m</div> <div>0 1 2 3 4m</div> <div>Dimensions in metres unless otherwise noted</div> | | | | | | | | |
| 03 | ISSUED FOR INFORMATION | A.D. | | 20.02.19 | | | | | | | | | | | |
| 02 | ISSUED FOR INFORMATION | A.D. | | 11.12.18 | | | | | | | | | | | |
| 01 | ISSUED FOR INFORMATION | A.D. | - | 12.10.18 | | | | | | | | | | | |
| No. | Revision Description | Initials | Approved | Date | | | | | | | | | | | |